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Application Number:	20/00619/FULM
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Application Type:	Full Planning Application (Major)
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Proposal Description:	Rural Leisure Centre including man-made lake, trim trail, tennis courts, cafe, changing facilities, shower/toilet block, camping and touring caravan pitches, 7 holiday log cabins and car
At:	Land South Of Hayfield Lane Auckley Doncaster DN9 3NP

For:	Tom Lewis
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Third Party Reps:	7 Letters of objection 7 Letters of support	Parish:	Auckley Parish Council
		Ward:	Finningley

Author of Report:	Alicia Murray
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SUMMARY

The proposal seeks full planning permission for the creation of a rural outdoor leisure facility, with an open water swimming activity lake, associated built form, log cabins, caravan/camping pitches, MUGA and access/car parking. The proposal is considered to be acceptable in policy terms being an appropriate use within the Countryside Policy Area and is fully justified to meet the policy requirements within the Local Plan and is therefore considered to be an acceptable and sustainable form of development in line with paragraph 7 and 8 of the National Planning Policy Framework (NPPF, 2021).

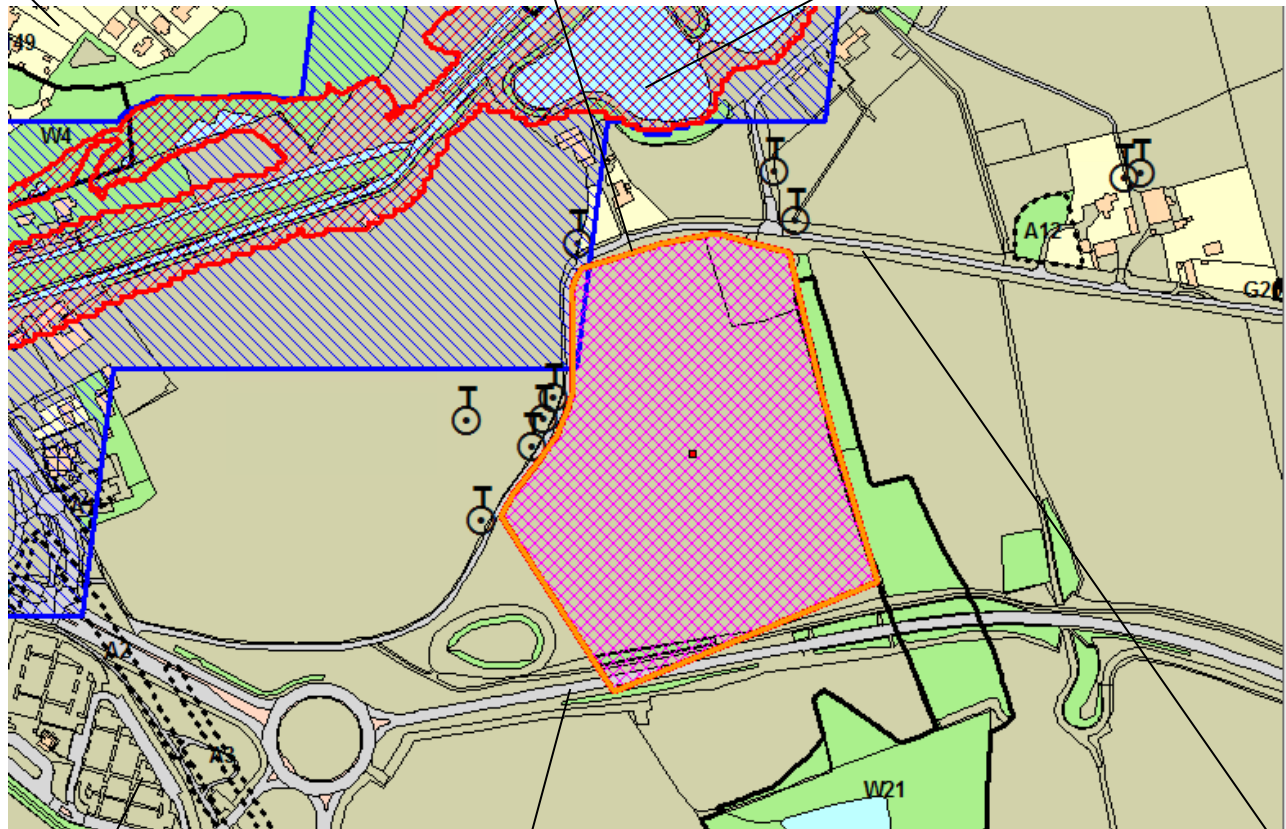
The report demonstrates that there are no material planning considerations that would significantly or demonstrably outweigh the social, economic or environmental benefits of the proposal in this location. The development would not cause undue harm to neighbouring properties, the highway network, local habitat/trees, the River Torne or the wider character of the area.

RECOMMENDATION: GRANT planning permission subject to conditions.

Bessacarr

Site Access

Hayfield Fisheries



Park and Ride

Great Yorkshire Way

Hayfield Lane leading to Hurst Lane and Auckley.

1.0 Reason for Report

- 1.1 The application is being presented to Members due to the level of public interest and the request from Councillor Steve Cox for the application to be called into planning committee. The reason for the call in request is due to Councillor Cox's concerns of highway impact specifically the impact with the Hurst Lane junction, and issues with water run and waste water.

2.0 Proposal and Background

- 2.1 Planning permission is sought for a rural leisure centre including a man-made lake, trim trail, tennis courts, café, changing facilities, shower/toilet block, camping and touring caravan pitches, 7 holiday log cabins; with associated works including access and car parking.
- 2.2 The application has been amended to overcome concerns raised by highways, trees, and policy. Further information has also been submitted to overcome concerns raised by drainage, environment agency, archaeology and ecology.
- 2.3 The access has been created prior to the application being granted, it has been confirmed by Planning Enforcement that the access is the same as that proposed on this application. The enforcement case is on hold until this application is determined.

3.0 Site Description

- 3.1 The site is a Greenfield parcel of land off Hayfield Lane, Auckley. The site is bound by hedgerow and the southern and western boundary are adjacent to Great Yorkshire Way (A638). The eastern boundary runs along an existing woodland. The northern boundary on Hayfield Lane which is a country road which leads to a dead end, along Hayfield Lane is existing dwellings and Hayfield Fishing Lakes.

4.0 Relevant Planning History

- 4.1 This site has no previous planning history.

5.0 Site Allocation

- 5.1 The site is identified within the Local Plan as Countryside Policy Area.

5.2 National Planning Policy Framework (NPPF 2021)

- 5.3 The National Planning Policy Framework 2021 (NPPF) sets out the Government's planning policies for England and how these are expected to be applied. Planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. The National Planning Policy Framework is a material consideration in planning decisions and the relevant sections are outlined below:
- 5.4 Paragraph 2 states that planning law requires applications for planning permission to be determined in accordance with the development plan, unless material considerations indicate otherwise.
- 5.5 Paragraphs 7 – 11 establish that all decisions should be based on the principles of a presumption of sustainable development.

- 5.6 Paragraph 47 reiterates that planning law requires that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise.
- 5.7 Paragraphs 55 and 56 states that Local Planning Authorities should consider whether otherwise unacceptable development could be made acceptable through the use of conditions or planning obligations. Planning obligations should only be used where it is not possible to address unacceptable impacts through a planning condition. Planning conditions should be kept to a minimum and only be imposed where necessary, relevant to planning and to the development to be permitted, enforceable, precise and reasonable in all other respects.
- 5.8 Part 6 of the National Planning Policy Framework (NPPF) (2021) 'Building a strong, competitive economy' considers the concept of supporting a prosperous rural economy.
- 5.9 Paragraph 84 provides specific guidance in regard of planning decisions for non-residential rural development stating that they should enable:
- a) "The sustainable growth and expansion of all types of business in rural area both through conversion of existing buildings and well-designed new buildings.
 - b) The development and diversification of agricultural and other land-based rural businesses.
 - c) Sustainable rural tourism and leisure developments which respect the character of the countryside; and
 - d) The retention and development of accessible local services and community facilities, such as local shops, meeting places, sports venues, open space, cultural buildings, public houses and places of worship."
- 5.10 Paragraph 85 sets out the need to recognise that in order to support local business and community needs of rural areas, on occasion existing settlement boundaries and public transport connection need cannot apply. In these circumstances, sensitivity needs to be taken in ensuring development does not produce negative externalities to its rural setting. This includes capitalising on any usage of sustainable transport options as to not overwhelm existing local road infrastructure. Further, it suggests that sites that have physical cohesion with existing settlements should be promoted for development if they provide sustainable growth opportunities.
- 5.11 Paragraph 111 states that development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.
- 5.12 Paragraph 174 states planning policies and decisions should contribute to and enhance the natural and local environment, including preventing new and existing development from being put at unacceptable risk from land instability.
- 5.13 Paragraph 183 states planning policies and decisions should ensure that a site is suitable taking account of ground conditions and any risks arising from land instability and contamination.
- 5.14 Paragraph 184 states where a site is affected by contamination or land stability issues, responsibility for securing a safe development rest with the developer and/or landowner.

Local Plan

- 5.15 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires proposals to be determined in accordance with the development plan unless material considerations indicate otherwise. The development plan for Doncaster consists of the Doncaster Local Plan (adopted 23 September 2021). The following Local Plan policies are relevant in this case:
- 5.16 Policy 13 relates to sustainable transport within new developments. Part A.6 states that proposals must ensure that the development does not result in an unacceptable impact on highway safety, or severe residual cumulative impacts on the road network. Developments must consider the impact of new development on the existing highway and transport infrastructure.
- 5.17 Policy 16 seeks to consider the needs of cyclists within new developments.
- 5.18 Policy 25 deals with what types of development are appropriate within the Countryside Policy Area.
- 5.19 Policy 30 deals with the need to value biodiversity.
- 5.20 Policy 32 states that the design process should consider woodlands, trees and hedgerows.
- 5.21 Policy 39 is with respect to Development Affecting Archaeology.
- 5.22 Policy 41 relates to character and local distinctiveness and states that development proposals will be supported where they recognise and reinforce the character of local landscapes and building traditions; respond positively to their context, setting and existing site features as well as respecting and enhancing the character of the locality. Developments should integrate visually and functionally with the immediate and surrounding area at a street and plot scale.
- 5.23 Policy 42 requires proposals to reflect and respect character and local distinctiveness. In all cases, the components of a development must be designed and assessed to ensure that, amongst other things, it provides safe and secure private property, public areas and the adoptable highway ensuring access points.
- 5.24 Policy 48 states that development will be supported which protects landscape character, protects and enhances existing landscape features, and provides a high quality, comprehensive hard and soft landscape scheme.
- 5.25 Policy 50 promotes strong, vibrant and healthy communities by supporting development that makes a positive contribution to forming a sustainable and healthy ethos within the community such as providing increased access to active travel options, the provision of good access to leisure facilities and green space, and development that supports and encourages healthy lifestyles
- 5.26 Policy 53 encourages the provision of new indoor recreation and leisure facilities where there is an identified need in the community, it enforces greater accessibility to the public, involves the usage of derelict or degraded land that is reclaimed and

retrofitted to revitalise communities, provides alternative and specialist sporting provision, is of an appropriate size and scale to the development's context, and is located in urban areas where it is accessible to a larger sample of the public.

- 5.27 Policy 55 deals with the need to mitigate any contamination on site.
- 5.28 Policy 56 requires the need for satisfactory drainage including the use of SuDS.
- 5.29 Policy 57 is with respect to flood risk management.
- 5.30 Policy 58 deals with low carbon and renewable energy within new developments.
- 5.31 Policy 60 seeks to protect and enhance Doncaster's soil and water resources.

Auckley Neighbourhood Plan (NP).

- 5.32 Auckley Neighbourhood Plan has been formally adopted (19th May 2023). This forms part of the development plan for Doncaster and substantial weight is given.
- 5.33 Policy APN9: deals with biodiversity and nature conservation, requesting net gain is achieved and priority habitats are preserved, restored and protection of priority species. The policy advice that proposals that cannot mitigate and compensate for the loss of a locally or nationally identified site of biodiversity value will not be supported.
- 5.34 Policy APN10: is regarding the protection of the landscape character and refers to developments being limited to the Local Plan Policy 25 and Policy 1.

Other material planning considerations and guidance

- 5.35 Doncaster Council's previous suite of adopted Supplementary Planning Documents (SPDs) have been formally revoked in line with Regulation 15 of the Town and Country Planning (Local Planning) (England) Regulations 2012, following the adoption of the Local Plan. The first SPD in line with the new Local Plan with respect to Biodiversity Net Gain was adopted in September 2022.
- 5.36 The Transitional Developer Guidance (April 2022) provides guidance on certain elements, including design, during the interim period, whilst new SPDs to support the adopted Local Plan are progressed and adopted. This guidance is attached limited weight.
- 5.37 Other material considerations include:
 - National Planning Practice Guidance (ongoing)
 - National Design Guide (2019)
- 5.38 Other Council initiatives include:
 - Doncaster Green Infrastructure Strategy 2014 – 2028
 - Doncaster Masterplan
 - Doncaster Delivering Together
- 5.39 Launched in September 2021, Doncaster Delivering Together (DDT) is the Council's new 10-year Borough Strategy. DDT is about everyone being able to

thrive and contribute to thriving communities and a thriving planet. This strategy does not form part of the adopted development plan, but it is important that the policies of the Doncaster Local Plan achieve the aims and objectives of DDT strategy. The DDT has identified 8 priorities to deliver for Doncaster over the next ten years.

1. Tackling Climate Change
2. Developing the skills to thrive in life and work
3. Making Doncaster the best place to do business and create good jobs
4. Building opportunities for healthier, happier and longer lives for all
5. Creating safer, stronger, greener and cleaner communities where everyone belongs
6. Nurturing a child and family - friendly borough
7. Building transport and digital connections fit for the future
8. Promoting the borough and its cultural, sporting and heritage opportunities

6.0 Representations

6.1 This application has been advertised in accordance with Article 15 of the Town and Country Planning Development Management Procedure (England) Order 2015 by means of site notice, council website, press advertisement and neighbour notification.

6.2 The application was initially submitted on the 01.04.2020 and advertised via site notice/neighbour letter on 24.04.2020 and press notice on 23.04.2020. Following this publicity, a total of 7 letters of objection were received. A summary of the issues raised is set out below:

- Works have commenced prior to permission being granted
- Storing asphalt on site and raising land levels
- Countryside Policy Area and loss of agricultural land
- Impact on natural habitat
- Land could be contaminated
- Foul drainage could pollute ground water supply
- Access is unsuitable, from Hayfield Lane which can only be accessed via Auckley. Hayfield Lane is a country lane and isn't suitable for pedestrians and cyclists. Auckley already impacted by the airport and Yorkshire Wildlife Park. Access would be better situated on Great Yorkshire Way
- Lack of information on environmental impact of the scheme
- Impact to great crested newts
- Chemicals in the lake could impact River Torne
- Already caravans and lakes on Hayfield Lane
- Increase in air pollution and noise pollution from potential further grid locked traffic in Auckley
- Toxic waste will go into freshwater system
- Safety for users of Hayfield Lane during construction with large vehicles accessing the site, as issues have already been raised since the application has been pending.

6.3 7 letters of support have been received; a summary of the matters raised is set out below:

- Gives people opportunity to partake in outdoor activities
- Improves health and wellbeing of users
- To get the same facilities people would have to travel outside of Doncaster
- Will bring people into Doncaster, improving tourism
- Good low key green option for both exercise and tourism in Doncaster
- Good accessible location
- Allows people to enjoy the countryside whilst partaking in sport/exercise
- Would like to see a good level of disabled accessibility and promotion of active travel
- Local paddle sport club outlines the need for an outdoor facility at the south of Doncaster to enable existing clubs to continue to operate successfully and expand
- Local triathlon club outlines need for an open water swimming location as the other option in the city is often unavailable and is hired out 5 days a week; other offer on site is also welcomed to improve the club training offer.

7.0 Parish Council

7.1 Auckley Parish Council have made 3 representations on this application, a summary of their objections is outlined below:

- Concerns regarding the amount of traffic generated by this development, Hurst Lane is a very busy road with traffic from the airport, college, school, quarry, Wildlife Park, new housing developments and the existing caravan park at Hayfield Lakes. Requested further traffic assessments to be completed to ascertain the in-Hayfield Lane traffic.
- Concerns over how construction will be managed
- Issues with large vehicles using Hayfield Lane due to its nature
- Questions how the horse livery will be managed (this element has been removed from the scheme)
- Insufficient foul drainage information
- Disabled parking bays do not meet the requirements.

8.0 Relevant Consultations

8.1 **Yorkshire Wildlife Trust** – Requested full ecological surveys and a bio-diversity net gain assessment be submitted.

8.2 **CDC Ecology** – Following the submission of an Ecological Impact Assessment and Bio-diversity Net Gain Assessment (inc. metric), the Ecology Officer does not object to the application subject to conditions relating to; management and monitoring of onsite habitats, and a construction environmental management plan.

8.3 **Architectural Liaison Officer**– Recommends a series of crime prevention methods be installed on site. Recommends security fencing be erected, a boundary treatment condition is placed on this decision.

8.4 **South Yorkshire Archaeology Service** – Following the submission of an archaeological evaluation, it is considered that the development has potential for

archaeological implications. Therefore, conditions are requested in relation to a Written Scheme of Investigation for archaeological mitigation.

- 8.5 **Anglian Water** – No connection to an Anglian water sewer, therefore no comments to make.
- 8.6 **Environment Agency** – Following the submission of further drainage calculations and justification the EA do not object to the application. Advice notes to the applicant are included as an informative on the decision relating to water discharge activity at the site.
- 8.7 **Severn Trent** – Given the foul is to be drained into a sewage treatment plant and surface water is to be discharged into a watercourse, Severn Trent have no objections to the proposal. Severn Trent have a note to the applicant, which has been added as an informative.
- 8.8 **Local Plans (Minerals)** – Requested further details of the volume of material to be extracted and the nature of the material. The agent has confirmed that 10,000 tonnes of material would be disposed off site over an 8-12 week extraction period to cut, fill and remove excess off site. The Minerals Policy Officer is satisfied that overall, the amount is not substantial and there are no objections to the application. No monitoring condition is required.
- 8.9 **CDC Trees** – No objections to the proposal, the Tree Officer requested the tree survey plan be plotted onto the site plan. This has been completed and the Tree Officer is satisfied, however, to ensure the bunding does not impact the RPAs of trees, conditions are requested to protect the Trees and to provide further landscaping details.
- 8.10 **CDC Drainage** – No objections subject to conditions for the land drainage details and SUDs details to be submitted. The Drainage Officer has provided some notes for the applicant, which have been included as an informative.
- 8.11 **CDC Environmental Health** – No objection. A proposal such as this is unlikely to generate little additional noise in an area that already has slightly elevated noise levels due to the close proximity of busy arterial roads.
- 8.12 **CDC Public Rights of Way** – The proposal does not affect any legally recorded public rights of way.
- 8.13 **CDC Highways** – following the submission of additional details, the Highways Officer does not object to the application. The officer has requested a number of conditions relating to site surfacing and construction traffic management.
- 8.14 **CDC Local Plans (Open Space)** – Further justification was requested for the proposal and why the relevant elements are required i.e. the café and toilet block locations. The Policy Officer does not object to the application subject to the conditioning of the ancillary uses being tied to the site users.
- 8.15 **CDC Contaminated Land** – No objections, requested conditions regarding a contaminated land risk assessment to be submitted prior to commencement of ground works.

- 8.16 **CDC Public Health** – No objections. If there are any changes to the proposed development that may impact negatively on health and wellbeing then Public Health may request further information.
- 8.17 **CDC Transportation** – No objections to the proposal, given the estimated trip generations of 68 a day, the level of traffic is not considered to be severe. The Transport Planner has requested conditions relating to cycle parking and EV charging points.
- 8.18 **Doncaster East Drainage Board** – The Board maintained, an open watercourse, exists 190m North of the site and to which BYELAWS and the LAND DRAINAGE ACT 1991 applies. The drainage board have also requested a surface water drain condition similar to that requested by the CDC Drainage Team.
- 8.19 **CDC Air Quality** – Following the submission of an air quality assessment, it is determined that the air quality impact will not be significant. The Air Quality Officer has requested a condition in relation to the submission of an air quality mitigation plan.
- 8.20 **Councillor Richard Alan Jones** – Objects to the application, raising concerns over caravans utilising Hayfield Lane, no safe passing places or footpaths along Hayfield Lane, and issues with land drainage including the hot tubs. The Councillor also raised concerns relating to equine uses but this element has been removed from the scheme. The Councillor also raised concerns about asphalt being deposited on site and the potential for this to infiltrate the Hayfield Lakes fishing site.
- 8.21 **Councillor Steve Cox** – Objects to the application, raising concerns of highway impact specifically the impact with the Hurst Lane junction, and issues with water run and waste water.

9.0 Assessment

9.1 The proposal seeks permission for a rural outdoor leisure centre including man-made lake, trim trail, tennis courts, cafe, changing facilities, shower/toilet block, camping and touring caravan pitches, 7 holiday log cabins, yurts and car parking. In considering the proposal the main material planning considerations are outlined below:

- The acceptability of the proposal
- The impact on the character and appearance of the area
- The impact on neighbouring residential properties
- The impact on the highway network and highways standards
- The impact on the trees and landscaping
- The impact on the ecology and wildlife of the site
- Flooding and Drainage issues
- Pollution
- Loss of agricultural land
- Archaeological Impact

9.2 For the purposes of considering the balance in this application the following planning weight is referred to in this report using the following scale:

- Substantial

- Considerable
- Significant
- Moderate
- Modest
- Limited
- Little
- No

EIA Development

- 9.3 Schedule 2 of the Environmental Impact Assessment (EIA) Regulations 2020 identifies development likely to have significant effects on the environment by virtue of factors such as its nature, size or location. It specifically identifies holiday villages outside urban areas and associated developments where the area of the development exceeds 0.5 hectare as being a Schedule 2 development. The application has been screened and it is considered that the development does not cause a significant environmental impact which requires an environmental impact assessment.

Appropriateness of the proposal

- 9.4 The NPPF supports a prosperous rural economy, including supporting both the sustainable growth and expansion of all types of business and enterprise in rural areas and rural tourism and leisure developments that benefit businesses in rural areas, communities and visitors which respect the character of the countryside. In my mind, this would include supporting the provision and expansion of tourist and visitor facilities in appropriate locations where identified needs are not met by existing facilities in rural service centres. Accordingly, there is support at a national level for the principle of this type of development in a countryside location.

- 9.5 The site is located within the Countryside Policy Area, which is defined in under Policy 1 as outside of the Green Belt and beyond defined 'Development Limits' as shown on the Policies Map. Proposals for new development in the Countryside Policy Area, including those which support the rural economy, will be supported where in accordance with Policy 25. Part 4 of Policy 25 states that proposals for non-residential developments will be supported in the Countryside Policy Area provided that:

A) the rural location of the enterprise is justifiable to support a prosperous rural economy in accordance with national policy in the NPPF;

B) the location of the enterprise would not have a significant adverse effect on neighbouring uses or on highway safety;

C) the development is of a size (including floorspace) and scale commensurate with an existing use, or that reasonably required for a new use, and with the rural character of the location; and

D) the scale and design of the proposal would not have a significant adverse impact on the landscape.

- 9.6 The supporting text to Policy 25 states the NPPF gives clear guidance that planning policies should support economic growth in rural areas, and together with other policies in the Plan, Part 4 of this policy provides support for appropriate proposals for new non-residential development in the Countryside Policy Area. These uses

may include the growth and expansion of rural businesses, the development and diversification of agricultural and land-based rural businesses and sustainable tourism and leisure developments.

- 9.7 Policy 25, Part 4 therefore allows sustainable development opportunities in the countryside to grow and expand an existing successful and unique tourism attraction that addresses local demand and provides accommodation in a well maintained, secure and attractive setting. The supporting text to the policy recognises that sites to meet local business and community needs in rural areas may need to be located adjacent to, or beyond existing settlement limits, where they can demonstrate that there is an overriding benefit to the local economy and/or community and/or environment for locating development away from built up areas.
- 9.8 The proposal has been substantially amended to create a more cohesive outdoor recreation scheme. The proposal is for a 'triathlon concept' site which offers open water swimming and a trim trail running route. The cycling would be on road or utilising the cycle track facility at Doncaster Dome 4.6 miles away from the site, there are sporadic cycle lanes on the route to and from the site to the cycle track. The nearest triathlon training facility is located at the Brownlee Centre in Leeds and there is one other open water swimming lake in Doncaster at Hatfield, 9 miles away from this site. However, supporting representations received suggest local triathlon clubs cannot easily access this facility for training sessions. It is therefore considered that there is a local demand for such facility in Doncaster. Business Doncaster are supportive of the proposal and have helped the applicant throughout the pre-application and planning application process.
- 9.9 The proposal includes a log cabins and caravan pitches to provide overnight accommodation and other ancillary services i.e. café, reception, maintenance storage, and MUGAs. All of these facilities are to support the open water activities, but it is also acknowledged that the accommodation and MUGA will be rented out separately. This will be required to financially support the overall site usage. However, a condition would be included to ensure the café etc. will be the for the users of the facility only and not for the general public.
- 9.10 The supporting text to Policy 25 also refers to supporting the development and diversification of agricultural and land-based rural businesses and sustainable tourism and leisure developments. This is consistent with the requirements of the NPPF and Paragraph 84 which states planning policies and decisions should enable 'sustainable rural tourism and leisure developments which respect the character of the countryside'.
- 9.11 The site is within a 5-minute walk of the local amenities on Hayfield Lane (across the road over Hurst Lane) and is close to the major road links in, out, and around Doncaster. Whilst being in the Countryside Policy Area, it is closely connected to nearby settlements and is considered to be in a sustainable location. Visitors could access the local area on foot or via bus, as there are bus stops on Hurst Lane approximately 3-minute walk away from the site. Furthermore, there would be good access to public rights of way for visitors to enjoy recreational walking and cycling opportunities. However, it is noted that given the nature of the proposed use, visitors are likely to travel to the site by car from their home addresses and bring with them what is required to enjoy their stay and their sports equipment. However, the onsite shop, café and local services within a short would reduce the reliance on the use of the private car.

- 9.12 The proposed development would provide an outdoor sport, recreation facility to promote tourism in a rural location. The site is considered to be in a sustainable location with good levels of accessibility and proximity to local services. Policy 25, Part 4 recognises that sites may need to be located adjacent to, or beyond existing settlement limits, where it can demonstrate that there is an overriding benefit to the local economy and/or community and/or environment for locating development away from built up areas. Given the nature of the use proposed there would be limited sites within the settlement limits to accommodate an open water activity lake. The scale of the development would not likely result in significant environmental effects in this regard and it is demonstrated that there is a demand for the development within the local area, furthermore the limited number of triathlon training facilities would make the development an attraction for the wider South Yorkshire area. The development is considered to accord with Policy 25, Part 4 (A), which supports sustainable tourist development where the rural location of the enterprise is justifiable.

Sustainability

- 9.13 The National Planning Policy Framework (NPPF 2021) sets out at paragraph 7 that the purpose of the planning system is to contribute to the achievement of sustainable development. At a very high level, the objective of sustainable development can be summarised as meeting the needs of the present without compromising the ability of future generations to meet their own needs
- 9.14 There are three strands to sustainability, social, environmental and economic. Para.10 of the NPPF states that in order sustainable development is pursued in a positive way, at the heart of the Framework is a presumption in favour of sustainable development.

SOCIAL SUSTAINABILITY

Impact on Residential Amenity

- 9.15 Policy 44(A) of the Local Plan states: Developments must protect existing amenity and not significantly impact on the living conditions or privacy of neighbours or the host property (including their private gardens), be over-bearing, or result in an unacceptable loss of garden space. Policy 54 (B) requires any the assessment of noise generating uses close to the site, and the potential noise likely to be generated by the proposed development. Policies 42 and 46 states, amongst other matters, that developments must have no unacceptable negative effects upon the amenity of neighbouring land uses or the environment.
- 9.16 The impact from the proposal is likely to stem from an increase in traffic and general activity which may potentially disturb other local residents to the north and east. The recreational nature of the proposal is not expected to be overly noisy subject to adequate controls by the site operator. Whilst there may be potential for outdoor noise to occur from groups it would still be subject to other, non-planning legislation. Suitable conditions could be imposed via this process, including the establishment of a management plan and complaints procedure, in the event of any unreasonable amounts of noise being created. The Environmental Health Officer (noise) has been consulted and no objections have been raised, furthermore they have not recommended any conditions. The Transportation Team are satisfied that number of daily trips (68) would not result in a severe harm to the highway network, additionally these trips are likely to be sporadic through out the day and therefore

would not result in detrimental harm to the neighbouring residents (approximately 35m away).

9.17 Conclusion on Social Impacts.

9.18 The nearest residential property is approximately 35m away opposite the site entrance. There are further properties to the east of the site. It is considered that given the nature of the use and the size of the site the impact to the residents would not be detrimental. Whilst it is acknowledged that there would be some nuisance caused during construction and during check in and check out times for the caravan pitches and log cabins, this impact is not considered to be detrimental. Furthermore, the Environmental Health Team have not objected to the application and deem the scale of the development to be at a level which would not require any planning control via condition. It is therefore considered that the development is in accordance with Policies 42, 44, 46, and 54 of the Local Plan.

9.19 ENVIRONMENTAL SUSTAINABILITY

Impact upon the character and appearance of the surrounding area

9.20 The site is an agricultural field within local landscape character area H2 Blaxton to Stainforth Heaths & Farmland as defined in the Doncaster Landscape Character Assessment Capacity Study. This is described as an area of the flat low lying Sherwood sandstone, with scattered farms with diversifying and recreational land uses. The site would meet the geometric form described in the study as a straight edged field with fragmented or missing hedge boundaries and major transport routes. This landscape is considered in the study to be of moderate quality, as its character is diverse and indistinct in areas. The study acknowledges that there are not many public rights of way so there will be limited numbers of recreational viewers, with less sensitive viewers travelling through the area via the roads and airport routes. I would agree that the land typography and interruption from the road would mean that the area of impact would be local rather than regional.

9.21 The study does not identify capacity for outdoor recreation and holiday accommodation, but a detailed housing survey identifies potential mitigation strategies to minimise the visual impact. This includes:

- Development edges to fit with the straight edges of the surrounding landscape features.
- Located adjacent to existing large settlements
- Locate where existing woodland and localised dips in the landform provide screening
- Scale of the development to fit the scale of land landscape elements such as fields and woodlands.
- Replace missing trees and hedgerows
- Located away from existing public rights of way to prevent enclosure and loss of existing views.
- Avoid merging settlements
- Use sustainable urban drainage schemes to incorporate developments into the surrounding landscape character.

9.22 The site neighbours' buildings dotted to the north, east and west, which are both housing and some commercial uses. There are 2 settlements to the north/west and

east of the site of Bessacarr and Auckley. However, the surrounding fields mainly to the south are clearly in active agricultural use, built form is of a low density and sporadic on all sides of the site, resulting in no clear boundary except that beyond Great Yorkshire Way to the south where there is no development.

- 9.23 The proposal would result in a 10-hectare new facility to provide up to 7 log cabins, 6 touring caravan pitches, alongside the open water lake, MUGA, and associated buildings. The development is placed around the site with the lake and cabins being central, the reception and maintenance building to the northern boundary next to the car parking. The development would feature a trim trail and bund around the boundary to the north, south and west and a 1.5m landscaping buffer to the east. A similar lake development is located to the north of the site which features a dwelling and restaurant establishment, this site also has permission for touring caravans. The proposed landscaping and bunding along with the existing hedgerow would limit the views of the site from the wider area, whilst also providing ecological enhancements. It is noted that the access, car parking, and main reception building would be visible from Hayfield Lane but the buildings are single storey to reduce the impact. Landscaping plans have been provided and the bunding has been moved outside of the RPA of the existing trees on site. The Tree Officer does not object to the application and the Ecology Officer is satisfied with the proposed landscaping scheme.
- 9.24 In conclusion, the development has been designed to protect the wider landscape and the scale of the buildings has been significantly reduced and justified as the minimum required for the use to successfully function. The landscaping and built form ensure the rural character is not detrimentally impacted by the development. The development is considered to be in accordance with the requirements of Policy 25, Part 4 (c) and (d) of the Local Plan and Policy ANP10 of the Auckley Neighbourhood Plan.

Highways/Traffic

- 9.25 Paragraph 110 of the NPPF states that development proposals should mitigate against any significant impacts on the transport network (in terms of capacity and congestion), or on highway safety.
- 9.26 Policy 13 (B) states new developments will need to provide, as appropriate, Transport Statements, Transport Assessments and Travel Plans to ensure the delivery of travel choice and sustainable opportunities for travel in line with the latest government guidance and best practice. The CDC Transport Planner requested the provision of trip generation figures as a requirement for this application, this has been provided.

Highway safety

- 9.27 Policies 13, 42 (B) and 46 (A) (2)(4) of the Local Plan states, amongst other things, that developments must provide safe and secure private property, public areas and the adoptable highway ensuring access points, street design, parking and operational highway requirements safely cater for pedestrians, cyclists and vehicles. There has been local concern with the impact of the proposal on highway safety, with visitors arriving and leaving the site leading from Hurst Lane (specifically the signalised junction) and down the country lane of Hayfield Lane.
- 9.28 Hayfield Lane can only be accessed via Hurst Lane now the Great Yorkshire Way link road has been completed, which has made Hayfield Lane a 'dead end' road.

Hayfield Lane is a country lane which is a single narrow lane with grass verges. It is also a well used route for pedestrians and dog walkers meaning cars tend to park along the lane. The lane is used to access all properties and businesses along the lane but can no longer be used as a through road. There are no parking restrictions on Hayfield Lane and there are no footpaths.

- 9.29 The site would be served by a single access point on Hayfield Lane, Hayfield Lane is a national speed limit road but given the single lane nature it is unlikely vehicles could reach national speed limit. Given the right-hand bend close to the site on Hayfield Lane the access point proposed is considered to be in the most appropriate location. Having the access opposite the Hayfield Lakes access would make it easier for vehicles to judge what other vehicles are making their way along Hayfield Lane, as there are no further buildings/business down Hayfield Lane towards Parrots Corner and the Hayfield Lakes access is also known to have caravans utilising it. Pedestrian and cycle access to the site will be provided via the proposed vehicular access, this is considered to be acceptable by the Highways Development Control Officer due to the provision of 4.8m access. In addition signage will be provided along Hayfield Lane directing visitors to the site and there would be temporary signage for the construction period to alert users of construction and for the construction vehicles.
- 9.30 Concerns have been expressed over car accidents in the vicinity of the site, number of vehicles using Hayfield Lane and the conflict between pedestrians and vehicles. The Highway Safety Team have been consulted and do not offer any objections. They have noted that traffic on Hayfield Lane has significantly reduced now the lane has been closed off at the Parrots Corner end as part of the Great Yorkshire Way scheme. The proposal has very low numbers of caravan pitches, tents and lodges (all being single figures) and the traffic numbers would not be significant. Furthermore, factoring in that most of the time caravans would arrive and go at the same time means even less chance to meet each other on the lane.
- 9.31 The internal layout of the site has been amended to provide sufficient car parking (39 spaces, 2 disabled bays at main carpark), delivery area, and wide enough access and parking for the caravan pitches and log cabin parking. It is concluded that that, subject to the proposed site access and internal roads being designed with appropriate highway design features then the proposals should not have a detrimental road safety impact on the local highway network and should not adversely affect the safety of other road users.
- 9.32 The Council's Development Management Highway Officer has been consulted on the application and, despite a number of design requirements to be altered, there are no fundamental principles which would indicate that a risk to highway safety would be severe.

Traffic and impact on the network

- 9.33 Policies (A) 13, 42 (B) and 46 (A) (2)(3) includes the requirement to make appropriate provision for access by sustainable modes of transport to protect the highway network from residual vehicular impact. The same policies consider the impact of new development on the existing highway and transport infrastructure. Where necessary, developers will be required to mitigate (or contribute towards) any predicted adverse effects on the highway and the wider transport network.

- 9.34 The vehicle trip generation of the proposed development has been projected at 68 trips on peak days and an average of 46.8 trips per day over the course of a week. Cycle parking is proposed by the main reception building along with EV charging points. The Council's Development Management Transportation Officer has been consulted on the application and, in summary, no objections have been raised. The development therefore complies with the above policies concerning the protection of highway safety.

Loss of Agricultural Land

- 9.35 Soils are classified from Grade 1 to 5 with Grade 1 and 2 being deemed to be the best and most versatile soils. Grade 3 soils have limitations that affect the choice of crops, timings, type of cultivation, harvesting and yield. The yield of more demanding crops grown on this land is generally lower or more variable than on Grade 1 and 2 soils.
- 9.36 Policy 60 of the Local Plan requires developments affecting land to comply with the following criteria

A) Proposals on non-allocated sites that involve the significant loss of the best and most versatile agricultural land (grades 1, 2 and 3a) will only be supported where:

1. there are no other suitable alternative locations on lower quality agricultural land (or non-agricultural land) available; or
2. the land can be reinstated back to its previous state (where possible).

The grade of the soil, based on the available information, involves Grade 3b - so not deemed to be within the best and most versatile soils classification. It is noted that there have been no objections from the Policy Officer on the matter, and much of the site could potentially be reinstated where acceptable. It is not felt that the application would be contrary to the provisions of Policy 60.

Flooding and Drainage

Flooding

- 9.37 Paragraph 159 of the NPPF states that inappropriate development in areas at risk of flooding should be avoided by directing development away from areas at highest risk (whether existing or future).
- 9.38 Policy 57 (A) states all development proposals will be considered against the NPPF, including application of the sequential test and, if necessary, the exception test.
- 9.39 The majority of the site lies within Flood Zone 1 according to the Environment Agency's maps, with the north-western corner being in flood zone 2. No development would be located within the small parcel of land in flood zone 2. Therefore, the application has been assessed as flood zone 1.
- 9.40 A Flood Risk Assessment has been submitted in support of the application. It states the site is of low risk from flooding and located on well drained land. The proposal intends to dispose of surface water into the existing proposed ponds which will provide multiple functions and blue infrastructure.

9.41 The development complies with Policy 57 being located within a low risk flood area.

Drainage

9.42 Policy 56 states proposals will be supported therefore in line with the following requirements:

A) There is adequate means of foul sewage disposal and treatment or that capacity can be made available in time to serve the development.

B) They will not increase flood risk on site and ensure no flooding to land or buildings elsewhere.

C) They achieve a reduction in surface water run off on brownfield sites, and no increase on existing rates for greenfield sites.

D) They secure the removal of culverting and avoid building over a culvert or new culverting of watercourses and a 10 metre buffer zone is left free from development from the water's edge;

E) They make use of Sustainable Drainage Systems unless it can be shown to be technically unfeasible.

F) They dispose of surface water appropriately according to the following networks in order of preference:

1. to an infiltration-based system wherever possible (such as soakaways).
2. discharge into a watercourse with the prior approval of the landowner and navigation authority (following treatment where necessary).

9.43 Policy 54 (D) requires an assessment of any adverse effects on the quantity, quality and ecology features of water bodies and groundwater resources, including contamination to Source Protection Zones.

9.44 There are no recorded sewers in the local area, therefore foul and surface water discharges will be required to be managed on site. The Drainage Strategy submitted outlines that foul water is proposed to be managed using a package treatment plant and discharged into the River Torne. Given the site is within the outer ground source protection zone, a permit will be required from the Environment Agency, the EA do not object to this drainage proposal and requested the applicant contact them to apply for the permit post decision. The package treatment and subsequent permit from the EA should ensure that the River Torne is not contaminated by this proposal.

9.45 Local Councillors and objectors have outlined concerns about the Hot Tubs and how they would be drained. The hot tubs proposed would be filled and emptied on a Monday, with Friday being check out day. The hot tubs chlorine levels would be checked but it would likely dissipate completely over the 2-day weekend. Once the chlorine has cleared, they will be emptied onto the grassed areas to drain slowly into the ground. This is considered acceptable by the Drainage Team, Environment Agency and local Drainage Board.

9.46 The drainage strategy concludes that there is significant storage capacity available on site up to a maximum of 13,500m³ to protect the development from flood water leaving the site. The storage capacity within the lake is 5930m³ before the lake would

overtop. A flow control chamber is proposed to control the discharge into a ditch on the eastern side of the site. The drainage system proposed would improve the overall run off rate on site post development and would prevent run off onto Hayfield Lane.

- 9.47 The Council's Drainage Team have reviewed the submission and offer no objections subject to conditions. The Environment Agency do not object to the amended drainage strategy and have requested conditions. Finally, the local water board and water company do not object to the application. The applicant is aware that separate permitting should be applied, and this must be done post submission. The proposal is considered to comply with Policy 58 of the Local Plan.

Trees and Landscaping

- 9.48 Policy 32 states proposals will be supported where it can be demonstrated that woodlands, trees and hedgerows have been adequately considered during the design process, so that a significant adverse impact upon public amenity or ecological interest has been avoided.
- 9.49 The main constraint on the proposal are the existing hedgerows and trees around the site perimeter. The site plans have been amended to accurately plot the existing trees on site. The proposal includes a 15m landscaping buffer along the eastern boundary, which would protect the hedgerow along that boundary. The proposal also includes bunding to the north, south and west boundaries, amended plans have been submitted to remove the bunding from the RPA of the existing trees. Both the landscaping buffer and bunding would set the development back from the boundaries which in turn improves the visibility of the proposal from the wider landscape. Additionally wild flower meadows, hedging with other soft landscaping are proposed within the buffer and around the site as a whole; along with a rewilding area to the south west corner. The finer details of the landscaping, including planting specifications shall be reserved by a pre-commencement planning condition, along with tree protection. Subject to those conditions, the Tree Officer does not object to the application. The proposed landscaping would soften the development, improving its setting within the rural location. It is considered that the proposal complies with Policy 32 of the Local Plan.

Ecology and Wildlife

- 9.50 Policy 26 requires the Council to protect, maintain, enhance and, where possible, extend or create Doncaster's green infrastructure. Policy 29 states proposals will only be supported which deliver a net gain for biodiversity and protect, create, maintain and enhance the Borough's ecological networks by:
- A) being of an appropriate size, scale and type in relation to their location within and impact on the ecological network;
 - B) maintaining, strengthening and bridging gaps in existing habitat networks;
 - C) planting native species and creating new, or restoring existing, national and local priority habitats and/or species; and
 - D) working with strategic partnerships to deliver conservation projects at a landscape scale where appropriate.
- 9.51 Policy 30 requires all applications to be considered against the mitigation hierarchy in accordance with National Policy. In line with best practice, the provision of compensation to account for residual biodiversity impacts will not be allowed unless the prior steps of the mitigation hierarchy have been followed, and all opportunities

to avoid and then minimise negative impacts have first been pursued. The Council use the DEFRA biodiversity metric to account for the impacts of a proposal on biodiversity and demonstrating that a net gain will be delivered. A minimum 10% net gain will be expected unless national standards increase this in the future.

- 9.52 The Ecological Impact Assessment has assessed the site in relation to protected species and has been found to be acceptable. Great Crested Newts have been assessed and are considered to be absent from the site. A BNG assessment has been found to be sound in principle and outlines a post construction baseline of 1.2ha of cropland to be lost and 5.77ha will be lost and replaced with ecological habitats of equal or great distinctiveness. The 0.72km of hedgerow will be protected during development as well the ancient woodland adjacent to the eastern boundary. The design includes the creation of new native species rich hedgerow with several native tree lines. The BNG assessment concludes that there would be a 53.2% habitat unit generation and 120.4% hedgerow units achieved post development. The development therefore complies with the above policies and Policy ANP9 of the Auckley Neighbourhood Plan.

Pollution

- 9.53 Policy 54 states development proposals that are likely to cause pollution, or be exposed to pollution, will only be permitted where it can be demonstrated that pollution can be avoided, or where mitigation measures (such as those incorporated into the design and layout of development) will minimise significantly harmful impacts to acceptable levels that protect health, environmental quality and amenity. Policy 54 (A) requires an assessment of the risks to public health to be provided.
- 9.54 The Council's Pollution Control Officer has reviewed the submitted assessments and considers that there would be no harm in terms of contamination subject to conditions relating to further verification reports being submitted, prior to commencement. The Environment Agency also do not object to the development with regards to pollution/land contamination. Comments made from residents and local members relating to the storing of Asphalt on the site, are noted but the applicant has a U1 exemption certificate for the storage of this product in connection to the construction of this proposal. Whilst it is acknowledged that this is presumptive of the applicant to assume planning permission would be granted and commence works (to the access only); if the application is refused by planning committee, then the material would have to be removed from site, as the applicant would be in breach of the U1 exemption. Furthermore, the applicant would have to apply separately for the access or return it to its former. This matter however is separate to this planning application and is dealt with by the Environment Agency.
- 9.55 Policy 54 (B) requires an assessment as to the presence of noise generating uses close to the site, and the potential noise likely to be generated by the proposed development. As noted above, no objections have been received from the Council's Environmental Health Officer and the final use of the site is unlikely to lead to unreasonable levels of noise.
- 9.56 Policy 54 (C) requires an assessment on the impact on national air quality; especially but not limited to Air Quality Management Areas, areas potentially close to the EU limit value, other sensitive areas and the aims and objectives of the Air Quality Action Plan. An Air Quality Assessment has been provided and found to be acceptable in principle. No objections have been received from the Council's Air Quality Officer, subject to a condition relating to a mitigation plan.

- 9.57 Policy 54 (D) deals with any adverse effects on the quantity, quality and ecology features of water bodies and groundwater resources, including contamination to Source Protection Zones. No objections have been received from the Environment Agency in this respect.

Archaeology

- 9.58 Policy 39 states that development affecting other archaeological assets will need to demonstrate how any benefits will outweigh harm to the site.
- 9.59 Trial trenching results have been submitted with the application to demonstrate the site's archaeological potential. This evaluation found a pit at the northern end of Trench 03 is a modern truncation, and an animal burial in Trench 12 could represent modern disposal of a natural mortality. The remaining features, ditches and pits recorded during the excavations contained no finds. While excavations did confirm the presence of features identified during the geophysical survey (2022) no finds were present, making it difficult to confirm the date or purpose of these features. The South Yorkshire Archaeology Service have been consulted and confirmed that the site has some archaeological potential and advises a written scheme of investigation be submitted prior to commencement of development to secure mitigation.

9.60 Conclusion on Environmental Issues

- 9.61 Para. 8 of the NPPF (2021) indicates, amongst other things, that the planning system needs to contribute to protecting and enhancing the natural built and historic environment, including making effective use of land, helping to improve biodiversity, using natural resources prudently, minimising waste and pollution, and mitigating and adapting to climate change, including moving to a low carbon economy.
- 9.62 In conclusion of the environmental issues, it is considered that there have been no significant issues raised which would weigh against the proposal that cannot be mitigated by condition. As such moderate weight attached to this in favour of the development through the achievement of BNG offsetting on site at a substantial level. The design, layout, and landscaping has been designed to be sympathetic to the rural setting of the site; there would be minimal impact to the landscape. The lake and the drainage systems proposed have been designed to protect the areas ground source water and to ensure outfall rates are improved. The Drainage Team and the Environment Agency do not object to the application, the proposal is not considered to result in an increased risk of flooding, nor would it result in harm to any person. The trip generation figures supplied are considered to be satisfactory to the Transport Planner and the access is suitable for the associated traffic with no objections raised by the Highways Network Management Team. Car parking is provided to an adequate level with sufficient manoeuvrability around the site, no objections are raised by the Highways DC Officer. Finally, whilst it is acknowledged that there could be some conflict with caravans accessing the site via the single track of Hayfield Lane, the numbers of caravan pitches are minimal to ensure the conflict is not severe.

9.63 ECONOMIC SUSTAINABILITY

9.64 It is anticipated that there would be some short term economic benefit to the development of the site through employment of construction workers and tradesmen connected with the build of the project however this is restricted to a short period of time and therefore carries limited weight in favour of the application. Whilst there may be some additional uplift for business within Auckley as a result of additional customers, this uplift is unknown and cannot be quantified at this time and so is afforded limited weight.

9.65 Conclusion on Economy Issues

9.66 Para 8 a) of the NPPF (2021) sets out that in order to be economically sustainable developments should help build a strong, responsive and competitive economy, by ensuring that sufficient land of the right types is available in the right places and at the right time to support growth, innovation and improved productivity; and by identifying and coordinating the provision of infrastructure.

9.67 Whilst the economic benefit of the proposal is slight and afforded only limited weight, it does not harm the wider economy of the borough and for that reason weighs in favour of the development.

10.0 PLANNING BALANCE & CONCLUSION

10.1 In accordance with Paragraph 11 of the NPPF (2021) the proposal is considered in the context of the presumption in favour of sustainable development. The principle of the development is considered to be acceptable and the proposal is in accordance with Policy 25 of the Local Plan, and this weighs considerably in favour of the application. In addition the proposed plans submitted with the application has shown that a suitable layout can be achieved that would be supportive of the character of the area, given its rural location and safeguard neighbouring properties and this weighs significantly in favour of the application.

10.2 The proposed access and car parking have been fully assessed by the Highways team and are considered to be acceptable. Furthermore, the trip generation and suitability of Hayfield Lane have also been assessed, and the impact is not considered to be severe. Along with the potential biodiversity net gain achieved by the development weigh moderately in favour of the application. The applicants have provided sufficient justification to the Environment Agency and the Council's Drainage Team meaning that no objections have been received subject to a suitably worded conditions; to ensure the development is adequately drained and the River Torne will remain uncontaminated.

10.3 Limited weight in favour of the application has been afforded to the potential economic benefits generated by the proposal.

10.4 The noise and traffic associated with equipment used during the construction of the site can be mitigated and controlled by condition and the short-term noise and disturbance associated with implementing the planning permission is considered to carry limited weight against the proposal.

11.0 RECOMMENDATION

11.1 MEMBERS RESOLVE TO GRANT PLANNING PERMISSION FOR THE PROPOSED DEVELOPMENT SUBJECT TO THE FOLLOWING CONDITIONS:

Conditions / Reasons

01. The development to which this permission relates must be begun not later than the expiration of three years beginning with the date of this permission.
REASON
Condition required to be imposed by Section 91(as amended) of the Town and Country Planning Act 1990.
02. The development hereby permitted shall be carried out in complete accordance with the details shown on the amended plans referenced and dated as follows:
Location Plan 024 00
Site Plan 026 Rev 15 - amended 07.06.2023
Highway and Pedestrian Movement Plans - 100 01 and 101 01
Lighting Plan 030 01 - amended 24.08.2022
Proposed Reception/changing building 023 02 - amended 02.11.2021
Proposed Camping Lodges and Cafe buildings 025 02 - amended 02.11.2021
REASON
To ensure that the development is carried out in accordance with the application as approved.
03. Any soil or soil forming materials brought to site for use in garden areas, soft landscaping, filling and level raising shall be tested for contamination and suitability for use on site. Proposals for contamination testing including testing schedules, sampling frequencies and allowable contaminant concentrations (as determined by appropriate risk assessment) and source material information shall be submitted to and be approved in writing by the LPA prior to any soil or soil forming materials being brought onto site. The approved contamination testing shall then be carried out and verification evidence submitted to and approved in writing by the LPA prior to any soil and soil forming material being brought on to site.
REASON
To secure the satisfactory development of the site in terms of human health and the wider environment and pursuant to guidance set out in the National Planning Policy Framework.
04. No development approved by this permission shall be commenced prior to a contaminated land assessment and associated remedial strategy, together with a timetable of works, being accepted and approved by the Local Planning Authority (LPA), unless otherwise approved in writing with the LPA.
 - a) The Phase 2 site investigation and risk assessment, if appropriate, must be approved by the LPA prior to investigations commencing on site. The Phase 2 investigation shall include relevant soil, soil gas, surface and groundwater sampling and shall be carried out by a

suitably qualified and accredited consultant/contractor in accordance with a quality assured sampling and analysis methodology and current best practice. All the investigative works and sampling on site, together with the results of analysis, and risk assessment to any receptors shall be submitted to the LPA for approval.

b) If as a consequence of the Phase 2 Site investigation a Phase 3 remediation report is required, then this shall be approved by the LPA prior to any remediation commencing on site. The works shall be of such a nature as to render harmless the identified contamination given the proposed end-use of the site and surrounding environment including any controlled waters, the site must not qualify as contaminated land under Part 2A of the Environment Protection Act 1990 in relation to the intended use of the land after remediation.

c) The approved Phase 3 remediation works shall be carried out in full on site under a quality assurance scheme to demonstrate compliance with the proposed methodology and best practice guidance. The LPA must be given two weeks written notification of commencement of the remediation scheme works. If during the works, contamination is encountered which has not previously been identified, then all associated works shall cease until the additional contamination is fully assessed and an appropriate remediation scheme approved by the LPA.

d) Upon completion of the Phase 3 works, a Phase 4 verification report shall be submitted to and approved by the LPA. The verification report shall include details of the remediation works and quality assurance certificates to show that the works have been carried out in full accordance with the approved methodology. Details of any post-remedial sampling and analysis to show the site has reached the required clean-up criteria shall be included in the verification report together with the necessary documentation detailing what waste materials have been removed from the site. The site shall not be brought into use until such time as all verification data has been approved by the LPA.

REASON

To secure the satisfactory development of the site in terms of human health and the wider environment pursuant to the National Planning Policy Framework.

This has to be prior to commencement so that any risks are assessed before works begin to the ground whether this be demolition works or construction works and remediation in place before works begin.

05.

The development hereby granted shall not be begun until details of the foul, surface water and land drainage systems and all related works necessary to drain the site have been submitted to and approved by the Local Planning Authority. These works shall be carried out concurrently with the development and the drainage system shall be operating prior to the occupation of the development.

REASON

To ensure that the site is connected to suitable drainage systems and to ensure that full details thereof are approved by the Local Planning Authority before any works begin.

06. No building hereby permitted shall be occupied until the sustainable drainage scheme for the site has been completed in accordance with the submitted details.
REASON
To ensure that the site is connected to suitable drainage systems, and to comply with current planning legislation - National Planning Policy Framework.
07. There shall be no piped discharge of surface water from the development prior to the completion of the approved surface water drainage works and no buildings shall be occupied or brought into use prior to completion of the approved foul drainage works.
REASON
To ensure that no foul or surface water discharge take place until proper provision has been made for their disposal.
08. No building shall be erected within 8 metres of the water course or culvert which passes through/runs adjacent to the site.
REASON
To ensure adequate access at all times and to protect the culvert from damage.
09. All surface water runoff from the site, excepting roof water, shall be discharged to the public surface water sewer/land drainage system or Highway Drain via a suitable oil/petrol/grit interceptor. Details of these arrangements shall be submitted to and approved by the Local Planning Authority prior to the commencement of the development and they shall be fully operational in accordance with the approved details before the site is brought into use.
REASON
To avoid pollution of the public sewer and land drainage system.
10. Prior to the development being brought into first use, an air quality mitigation plan shall be submitted to and approved in writing by the local planning authority. This plan shall calculate damage costs and demonstrate how they have been utilised to offset vehicle emissions during the lifetime of the development. Measures in any mitigation plan shall be in addition to those provided as a requirement for other planning matters. The mitigation plan shall be implemented before the development has been brought into use.
REASON
To contribute towards a reduction in emissions in accordance with air quality objectives and providing sustainable travel choice in accordance with policies CS9 and CS18 of the Doncaster Council Core Strategy.
11. Prior to the commencement of development, a Management and Monitoring Plan for proposed onsite habitats shall be submitted to the Local Planning Authority for approval in writing. The Management Plan shall be based on the proposals set out in the Ecological Impact Assessment EclA0 (ADAS, Ref 1051808 - WNT69105-963 (01) 03/11/22) and the Biodiversity Net Gain Assessment Metric 3.1 (02/06/22) and will contain the following details:

- o The baseline biodiversity assessment against which an uplift in biodiversity unit value will be monitored.
- o The project's biodiversity unit targets.
- o A detailed adaptive management plan setting out how habitats will be created or enhanced and describing the proposed ongoing management for a minimum of 30 years.
- o The details of when target condition will be achieved and how it shall be maintained.
- o A detailed monitoring plan that will be used to inform any potential changes to the ongoing management and assess the progress towards achieving target condition. This should outline the surveys that will be used to inform condition monitoring reports. Monitoring reports will be provided to the Local Planning Authority by the end of years 1,2,5,10,20, and 30 of the monitoring period.
- o The roles, responsibilities and professional competencies of the people involved in implementing and monitoring the biodiversity net gain delivery.
- o Evidence that the necessary resources are available to deliver the proposed biodiversity net gain plan and the ongoing management.

Once approved the Management and Monitoring Plan shall be implemented in accordance with the approved details in full and any subsequent changes to management as a result of findings from the monitoring agreed in writing with the Local Planning Authority.

REASON

To fulfil specifically the requirements of Local Plan policy 30B and enhance local ecological networks in accordance with Local Plan policy 29.

12.

Before the commencement of development a Construction Environmental Management Plan shall be submitted to the LPA for approval, and then implemented prior to the operation of the facility in accordance with the details as set out in the Ecological Impact Assessment (EclA) (ADAS, Ref 1051808 - WNT69105-963 (01) 03/11/22 0 and includes:

- A risk assessment of the potentially damaging construction activities in relation to wildlife and habitats.
- A method statement for the protection of reptiles and other terrestrial fauna that identified in the EclA that may be encountered on site.
- Measures to protect the adjacent Local Wildlife Site, 4.32- Hurst Plantation, Savage Brooks and Marr Flatts Wood (Local Wildlife Site) The use of protective fencing, exclusion barriers and wildlife safety measures.

REASON

To ensure the ecological interests of the site are maintained in accordance with Local Plan policy 29

13. No development shall take place on the site until a detailed soft landscape scheme has been submitted to and approved in writing by the Local Planning Authority. The soft landscape scheme shall include a soft landscape plan; a schedule providing plant and tree numbers and details of the species, which shall comply with section 8 Landscape, Trees and Hedgerows of the Council's Development Guidance and Requirements Supplementary Planning Document, nursery stock specification in accordance with British Standard 3936: 1992 Nursery Stock Part One and planting distances of trees and shrubs; a specification of planting and staking/guying; a timescale of implementation; and details of aftercare for a minimum of 5 years following practical completion of the landscape works. The trees shall be container grown or root balled and of minimum Extra Heavy Standard (14-16cm) size in accordance with table 1 of British Standard 3936-1: 1992 Nursery Stock. The pots of containerised trees must be proportionate to the size of the tree in accordance with table D4 of British Standard 8545: 2014 Trees: From nursery to independence in the landscape - Recommendations (BS8545) and the rootball of rootballed trees in accordance with table D5 of British Standard 8545. The trees shall be handled in accordance with 'Handling and Establishing Landscape Plants' by the Committee of Plant Supply & Establishment (1995) published by the Joint Council for Landscape Industries and/or section 9 Handling and Storage and Annexe E of BS8545. Thereafter the landscape scheme shall be implemented in full accordance with the approved details and the Local Planning Authority notified in writing within 7 working days to approve practical completion of any planting within public areas or adoptable highway within the site. Soft landscaping for any individual housing plot must be implemented in full accordance with the approved scheme, prior to occupation of the dwelling, which will be monitored by the Local Planning Authority. Any part of the scheme which fails to achieve independence in the landscape, or is damaged or removed within five years of planting shall be replaced during the next available planting season in full accordance with the approved scheme, unless the local planning authority gives its written approval to any variation.

REASON

In the interests of environmental quality and in accordance with Policy 48 of the Local Plan.

14. Before the development is brought into use, that part of the site to be used by vehicles shall be surfaced, drained and where necessary marked out in a manner to be approved in writing by the local planning authority.

REASON

To ensure adequate provision for the disposal of surface water and ensure that the use of the land will not give rise to mud hazards at entrance/exit points in the interests of public safety.

15. The vehicle turning space as shown on the approved plans shall be constructed before the development is brought into use and shall thereafter be maintained as such.

REASON

To avoid the necessity of vehicles reversing on to or from the highway and creating a highway hazard.

16. Before the development hereby permitted is brought into use, the parking as shown on the approved plans shall be provided. The parking area shall not be used otherwise than for the parking of private motor vehicles belonging to the occupants of and visitors to the development hereby approved.

REASON

To ensure that adequate parking provision is retained on site.

17. No construction works shall take place until full details of offsite highway works have been submitted to and agreed in writing by the Local Planning authority within a Construction Traffic Management Plan. The development shall be carried out in accordance with the agreed details and cover the following points, expanded on as required

- o Volumes and types of construction vehicles
- o identification of delivery routes;
- o identification of agreed access point
- o Contractors method for controlling construction traffic and adherence to routes
- o Size, route and numbers of abnormal loads
- o Swept path analysis (as required)
- o Construction Period
- o Temporary signage
- o Wheel Wash facilities
- o Timing of deliveries

REASON

To safeguard the living conditions of neighbouring residents and in the interests of highway safety

18. The cafe and shop uses hereby approved shall only be used for customers of the open water lake, tennis courts, or guests of the log cabins, caravan pitches and campers; and shall not at any time serve the general public.

REASON

To restrict the use to the needs of the fisheries only in the interests of the protection of highway safety and the Countryside Policy Area.

19. The development hereby permitted shall not be commenced until details of secure cycle parking facilities for the occupants of, and/or visitors to the development have been submitted to and approved in writing by the local planning authority. These facilities shall be fully implemented and made available for use prior to the occupation of the development hereby permitted and shall thereafter be retained for use at all times.

REASON

To ensure that satisfactory facilities for the parking of cycles are provided and to encourage travel by means other than private motor vehicles and to comply with policy 13 of the Doncaster Local Plan.

20. Prior to the occupation of the development hereby approved, details of electric vehicle charging provision shall be submitted to and approved in writing by the local planning authority. Installation shall comply with current guidance/advice. The development shall not be brought into use until the approved connection has been installed and is operational and shall be retained for the lifetime of the development. The development shall be carried out in accordance with the approved details.
REASON
To contribute towards a reduction in emissions in accordance with air quality objectives and providing sustainable travel choice in accordance with policy 13 of the Doncaster Local Plan.
21. No more than 7 caravan pitches, 7 log cabins shall be stationed on the site at any one time. The location of the overnight accommodation shall be limited to that shown on the approved Site Plan ref: 026 Rev 15 for the lifetime of the development.
REASON
In the interests of highway safety and local amenity as required by Policy 13, 42 and 55 of the Doncaster Local Plan
22. This permission authorises the use of the site for touring caravans, log cabins and camping pitches for holiday/recreational purposes only. No overnight accommodation at the site shall be occupied as any person's sole or main residence.
REASON
To ensure that the development is carried out in accordance with the application as approved and to ensure that permanent accommodation is restricted in line with Policy 25 of the Local Plan.
23. Prior to the commencement of the relevant works, details of the proposed external materials shall be submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with the approved materials.
REASON
To ensure that the materials are appropriate to the area in accordance with policy 42 of the Doncaster Local Plan.
24. Prior to commencement of the relevant works the full details of the trim trail equipment and its management and maintenance shall be submitted to and approved in writing by the Local Planning Authority, the trim trail equipment shall be fully installed prior to first use of the development hereby granted. The trim trail equipment shall be replaced or repaired if damaged or removed and retained for the lifetime of the development.
REASON
In the interests of environmental quality and Local Plan Policy 48.
25. Prior to the commencement of the development hereby granted a scheme for the protection of the root protection areas of all trees shown for retention on the approved plan (ref. Proposed Site Plan Drawing No. 026 Rev. 15 dated 01.06.2023) that complies with clause 6.2 of British Standard 5837: 2012 Trees in Relation to Design, Demolition and Construction - Recommendations shall be submitted

to and approved in writing by the Local Planning Authority. Tree protection shall be implemented on site in accordance with the approved details and the local planning authority notified of implementation to approve the setting out of the tree protection scheme before any equipment, machinery or materials have been brought on to site for the purposes of the development. Thereafter, all tree protection shall be maintained in full accordance with the approved details until all equipment, machinery and surplus materials have been removed from the site, unless the local planning authority gives its written approval to any variation. Nothing shall be stored or placed in any area fenced in accordance with this condition and the ground levels within those areas shall not be altered, nor shall any excavation be made, without the written consent of the Local Planning Authority.

REASON:

To ensure that all trees are protected from damage during the earthworks for the bunding and wider construction in accordance with Local Plan Policy 32.

27. Prior to the commencement of the relevant works, details of the proposed yurts and toilet block shall be submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with the approved details and shall be fully operational prior to the first use of the facility.

REASON

To ensure that the materials are appropriate to the area in accordance with policy 42 of the Doncaster Local Plan.

28. Prior to the commencement of any part of the development hereby approved, the following information shall be submitted to and approved in writing by the Local Planning Authority:

- (i) a full site survey showing:
 - o the datum used to calibrate the site levels
 - o levels along all site boundaries
 - o levels across the site at regular intervals

(ii) full details of the proposed finished floor levels of all buildings and proposed levels for external areas, specifically the bunding shown on the approved site plan.

The development shall be implemented only in full accordance with the approved details.

REASON

Prior approval of such details is necessary as they are fundamental to the initial site preparation works and to ensure that the details of the development are satisfactory having regard to Policy 25, Policy 42 and Policy 48 of the Doncaster Local Plan.

29. The flood lighting FL01 as shown on the approved lighting plan (ref; 030 01) shall only be illuminated during the operational hours until no later than 21:00 when the MUGA is not in use by customers the lights shall be switched off.

REASON

To protect the rural landscape, in accordance with Policy 25 and Policy 42 of the Local Plan.

30.

Prior to the first occupation of the development hereby approved, details of the drainage management and maintenance plan shall be submitted to and approved in writing by the Local Planning Authority. The drainage system for foul and surface water drainage shall be implemented prior to the first use of the facility, retained, managed and maintained for the lifetime of the development in accordance with the approved drainage management and maintenance plan.

REASON:

To ensure the drainage apparatus of the site is adequately maintained for the lifetime of the development and to accord with Para. 169 c) of the NPPF (2021).

31

Part A (pre-commencement)

No development, including any demolition and groundworks, shall take place until the applicant, or their agent or successor in title, has submitted a Written Scheme of Investigation (WSI) that sets out a strategy for archaeological investigation and this has been approved in writing by the Local Planning Authority. The WSI shall include:

- The programme and method of site investigation and recording.
- The requirement to seek preservation *in situ* of identified features of importance.
- The programme for post-investigation assessment.
- The provision to be made for analysis and reporting.
- The provision to be made for publication and dissemination of the results.
- The provision to be made for deposition of the archive created.
- Nomination of a competent person/persons or organisation to undertake the works.
- The timetable for completion of all site investigation and post-investigation works.

Part B (pre-occupation/use)

Thereafter the development shall only take place in accordance with the approved WSI and the development shall not be brought into use until the Local Planning Authority has confirmed in writing that the requirements of the WSI have been fulfilled or alternative timescales agreed.

REASON

To ensure that any archaeological remains present, whether buried or part of a standing building, are investigated and a proper understanding of their nature, date, extent and significance gained, before those remains are damaged or destroyed and that knowledge gained is then disseminated.

Informatives

01. It will be expected that any waste water treatment packages comply Building Regulation 2000 Part H and the following:
- The discharge from the waste water treatment plant should be sited at least 10m away from watercourses and any other buildings.
 - Packaged treatment works should be type-tested in accordance with BS 7781 or otherwise tested by a notified body.

02. The following information should be supplied when discharging the drainage conditions:

1. Records indicate that this area may be susceptible to surface water flooding. Please see Environment Agency indicative SW Flood Maps.

2. Surface water drainage plans should include the following:

- Rainwater pipes, gullies and drainage channels including cover levels.
- Inspection chambers, manholes and silt traps including cover and invert levels.
- Pipe sizes, pipe materials, gradients and flow directions.
- Soakaways, including size and material.
- Typical inspection chamber / soakaway / silt trap and SW attenuation details.
- Site ground levels and finished floor levels.

3. Surface Water Discharge From Greenfield Site

The total surface water discharge from greenfield sites should be limited to green field run-off rates - up to 1 in 100 years storm + climate change. On site surface water attenuation will be required.

If the greenfield run-off for a site is calculated at less than 2 l/s then a minimum of 2 l/s can be used (subject to approval from the LPA)

4. On Site Surface Water Management

The site is required to accommodate rainfall volumes up to 1 in 100 year return period (plus climate change) whilst ensuring no flooding to buildings or adjacent land.

The applicant will need to provide details and calculations including any below ground storage, overflow paths (flood routes), surface detention and infiltration areas etc to demonstrate how the 100 year + 30% CC rainfall volumes will be controlled and accommodated.

Where cellular storage is proposed and is within areas where it may be susceptible to damage by excavation by other utility contractors, warning signage should be provided to inform of its presence. Cellular storage and infiltration systems should not be positioned within highway.

Guidance on flood pathways can be found in BS EN 752.

5. If infiltration systems are to be used for surface water disposal, the following information must be provided:

- Ground percolation tests to BRE 365.
- Ground water levels records. Minimum 1m clearance from maximum seasonal groundwater level to base of infiltration compound. This should include assessment of relevant groundwater borehole records, maps and on-site monitoring in wells.
- Soil / rock descriptions in accordance with BS EN ISO 14688-1:2002 or BS EN ISO 14689-1:2003
- Volume design calculations to 1 in 30 year rainfall + 30% climate change standard. An appropriate factor of safety should be applied to the design in accordance with CIRIA C753 - Table 25.2.
- Location plans indicating position (Soakaways serving more than one property must be located in an accessible position for maintenance). Soakaways should not be used within 5m of buildings or the highway or any other structure.
- Drawing details including sizes and material.
- Details of a sedimentation chamber (silt trap) upstream of the inlet should be included.

Soakaway detailed design guidance is given in CIRIA Report 753, CIRIA Report 156 and BRE Digest 365.

6. The proposed development is within a groundwater source protection zone (SPZ_)

Where the development lies within SPZ 1 or 2, the applicant is advised to consult with the Environment Agency to ensure that pollution risk to aquifers is minimised.

All necessary precautions should be taken to avoid any contamination of the ground and thus groundwater. Guiding principles on the protection of groundwater are set out in Environment Agency document GP3.

7. The design of flow control devices should, wherever practicable, include the following features:

- a) Flow controls may be static (such as vortex flow controls or fixed orifice plates) or variable (such as pistons or slide valves);
- b) Controls should have a minimum opening size of 100 mm chamber, or equivalent;
- c) A bypass should be included with a surface operated penstock or valve; and
- d) Access should be provided to the upstream and downstream sections of a flow control device to allow maintenance.

03.

To discharge any drainage conditions which we have applied additional photographic evidence of the 'small ditch' which is present to along the eastern boundary is required to show that it is maintained condition and can accept additional surface water discharge. CCTV of culvert under site entrance 375mm diameter (if existing) and 550mm diameter culvert beneath Hayfield Lane to be provided to show they are in good condition.

Consent for watercourse alterations (headwall installation) is required. If the access culvert (375mm diameter) is proposed consent will also be required for this and construction details will need to be submitted.

Maintenance plan for drainage systems is required and must include watercourse maintenance as this will be a riparian watercourse.

Acceptance would be required from the Environment Agency or sewerage undertaker for the clay lined pond proposal and car park surface water treatment drainage strategy due to the location of Source Protection Zone and Groundwater Safeguard Zones, as per their response and current objection to the sites proposals:

'Water Resources

In our previous consultation response, we also highlighted that no further consumptive groundwater abstraction licences will be granted from the sandstone underlying the site due to the amount of environmental pressures and existing water demand in the area. We therefore requested further information on the construction and subsequent maintenance of the lake. It is now understood from the Drainage Calculations Report that the pond will be lined with the exception of the top 1m of the lake.

It is still unclear on whether construction of the lake would require dewatering of groundwater and, as the lake will be lined, how the site proposes to maintain water levels in the lake during dry periods. We are concerned that the absence of a source of top-up water may inhibit the viability of the development and so these details should be provided at the application stage.'

Landowner agreement would need to be provided for the foul drain which is to be installed on land outside of the redline boundary, the Environmental Permit from the Environment Agency would also be required to discharge foul into the watercourse from the site. Any of their concerns/objections for the discharge of foul with regards to the Source Protection Zone and Groundwater Safeguard Zones would need to be met.

Information would need to be provided to state why discharge of foul to watercourse is preferred over discharge to public sewer and the reasons connection to public sewer has been discounted for the site.

No development should take place within 10m from the top of watercourse bank adjacent to the site, as per Doncaster Council Local Plan - this includes the installation of fences. A 10m offset distance should be shown on the drainage plan. Any proposed footpaths within 10m of the top of bank will require details to be submitted to ensure that it will not damage the bank or affect maintenance access to the watercourse.

04. Environmental permit (Water Discharge activity) - advice to applicant
This development will require a permit under the Environmental Permitting (England and Wales) Regulations 2016 from the Environment Agency for a water discharge activity. We recommend that the applicant contacts our National Customer Contact Centre on 03708 506 506 to discuss the issues likely to be raised.

For further guidance please visit Discharges to surface water and groundwater: environmental permits - GOV.UK (www.gov.uk)

Please note that all applications are assessed on an individual basis and being granted a permit is not guaranteed.

No sewage effluent shall be discharged to any surface water without the benefit of an environmental permit.

No discharge of anything other than uncontaminated surface water shall be discharged to surface water or groundwater without the benefit of an environmental permit. This includes during the development, construction, and operational phases of the proposal. Particular consideration should be given to minimising the risk of silt pollution during the construction phase when removing topsoils.

05. Water Resources abstraction licence
During our two previous consultation responses, we have made it clear that no further consumptive groundwater abstraction licences will be granted in this area (other than exempt abstractions), due to existing environmental pressures and water demand on the underlying sandstone aquifer.

The latest submission indicates that the lake will be lined with the exception of the top 1m, and that the lake will receive surface water which drains from the development. We do not see any information on the need to maintain water levels within the lake, and therefore we have no further comment on this matter but refer the applicant to the Water Resources informative comment below. However, if a source of water is required to maintain water levels within the lake, we strongly advise the applicant to think carefully about how this could be achieved given the restrictions on abstraction licences discussed above.

06. Water Resources informative

You do not need a licence or to register an exemption for abstractions of 20 cubic metres or less a day, provided your abstraction is part of a single operation - if you abstract from the same source at multiple points, the exemption only applies if the combined total of all abstractions is 20 cubic metres or less a day.

For more information on abstraction licences, visit the gov.uk website at:

<https://www.gov.uk/guidance/check-if-you-need-a-licence-to-abstract-water>

07. Environmental permit (Flood Risk Activity) - advice to applicant

The River Torne is a designated main river. The amended plans suggest a new outfall will be required for the discharge of foul effluent to the River Torne.

The Environmental Permitting (England and Wales) Regulations 2016 require a permit to be obtained for any activities which will take place:

- o on or within 8 metres of a main river (16 metres if tidal)
- o on or within 8 metres of a flood defence structure or culverted main river (16 metres if tidal)
- o on or within 16 metres of a sea defence
- o involving quarrying or excavation within 16 metres of any main river, flood defence (including a remote defence) or culvert
- o in a floodplain more than 8 metres from the riverbank, culvert, or flood defence structure (16 metres if it's a tidal main river) and you don't already have planning permission

For further guidance please visit <https://www.gov.uk/guidance/flood-risk-activities-environmental-permits> or contact our National Customer Contact Centre on 03708 506 506. The applicant should not assume that a permit will automatically be forthcoming once planning permission has been granted, and we advise them to consult with us at the earliest opportunity.

08. Severn Trent Water advise that although our statutory sewer records do not show any public sewers within the area you have specified, there may be sewers that have been recently adopted under, The Transfer Of Sewer Regulations 2011. Public sewers have statutory protection and may not be built close to, directly over or be diverted without consent and you are advised to contact Severn Trent Water to discuss your proposals. Severn Trent will seek to assist you obtaining a solution which protects both the public sewer and the building.
09. Works carried out on the public highway by a developer or anyone else other than the Highway Authority shall be under the provisions of Section 278 of the Highways Act 1980. The agreement must be in place before any works are commenced. There is a fee involved for the preparation of the agreement and for on-site inspection. The applicant should make contact with Malc Lucas - Tel 01302 735110 as soon as possible to arrange the setting up of the agreement.

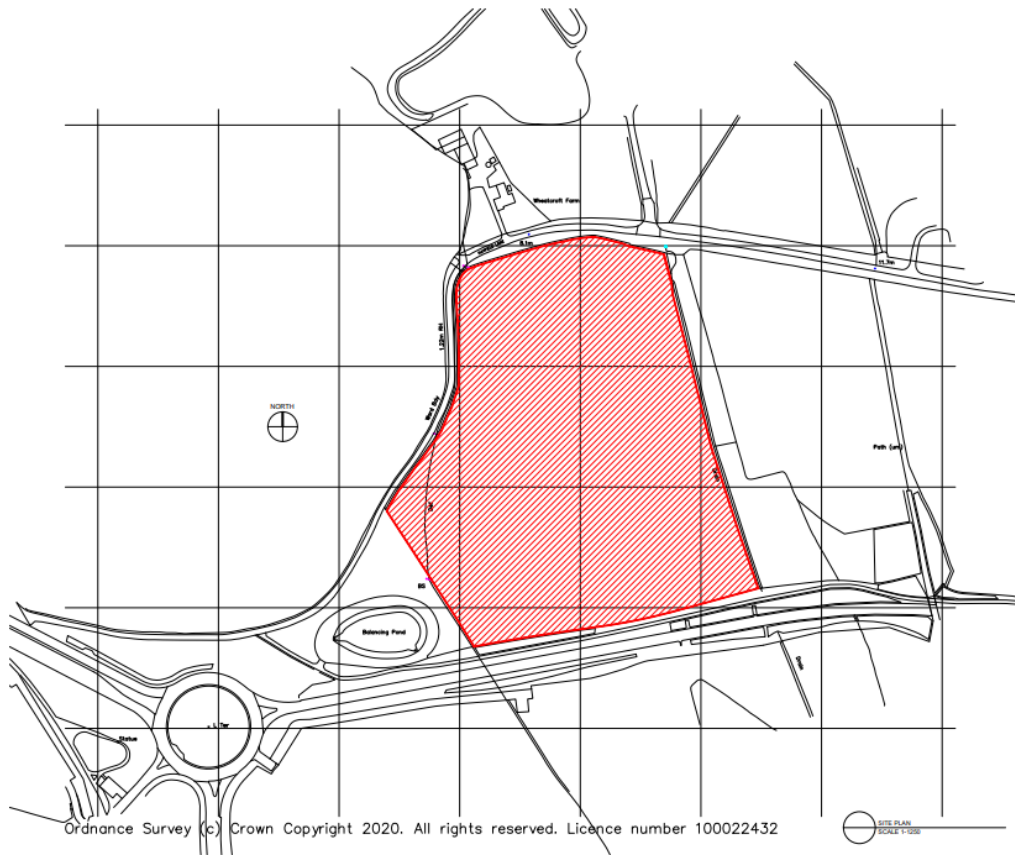
STATEMENT OF COMPLIANCE WITH ARTICLE 35 OF THE TOWN AND COUNTRY DEVELOPMENT MANAGEMENT PROCEDURE ORDER 2015

In dealing with the application, the Local Planning Authority has worked with the applicant to find solutions to the following issues that arose whilst dealing with the planning application:

- Additional information in relation to Air Quality, Ecology, Trees, principle of the development, and Drainage.
- Amendments to the plans to create a more cohesive proposal
- Additional highway information, including tracking and traffic numbers
- Additional information supplied to overcome archaeological concerns

The above objections, consideration and resulting recommendation have had regard to Article 8 and Article 1 of the First Protocol of the European Convention for Human Rights Act 1998. The recommendation will not interfere with the applicant's and/or objector's right to respect for his private and family life, his home and his correspondence

Appendix 1: Location Plan



NO.	DESCRIPTION	DATE	BY

DRAWN BY M T LEWIS		
PROJECT NAME NEW SHIFFIELD LANE TRIATHLON FACILITY		
DRAWING TITLE SITE LOCATION PLAN		
DRAWN BY AUTHOBY DESIGN ARCHITECTS 2020		
SCALE 1:1250@A1		
PROJECT NO. 24	DRAWING NO. 00	

Appendix 2: Site Plan

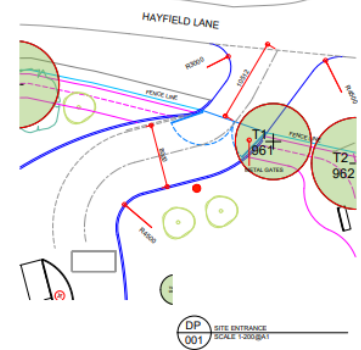
ACCOMMODATION SCHEDULE

RECEPTION AND CHANGE	395 M SQ	1 NO PERMANEN
CAFE	60 M SQ	1 NO PERMANENT
CAMPING LODGE	336 M SQ	7 NO PORTABLE
2 bed allowed for in schedule		
TOILET AND SHOWER	48 M SQ	1 NO PORTABLE
TOTAL	839 M SQ	

Area allocated for camping with electric hook up and access to toilet and shower facilities north of the site in main facilities building and south with shared facilities with Yurts.



NO	TYPE	DESCRIPTION	NO	NO
01	RECEP	RECEPTION AND CHANGE	1	01
02	CAFE	CAFE	1	02
03	CLDGE	CAMPING LODGE	7	03
04	TOILET	TOILET AND SHOWER	1	04
05	YURT	YURT	1	05
06	STAIR	STAIR	1	06
07	STAIR	STAIR	1	07
08	STAIR	STAIR	1	08
09	STAIR	STAIR	1	09
10	STAIR	STAIR	1	10
11	STAIR	STAIR	1	11
12	STAIR	STAIR	1	12
13	STAIR	STAIR	1	13
14	STAIR	STAIR	1	14
15	STAIR	STAIR	1	15
16	STAIR	STAIR	1	16
17	STAIR	STAIR	1	17
18	STAIR	STAIR	1	18
19	STAIR	STAIR	1	19
20	STAIR	STAIR	1	20
21	STAIR	STAIR	1	21
22	STAIR	STAIR	1	22
23	STAIR	STAIR	1	23
24	STAIR	STAIR	1	24

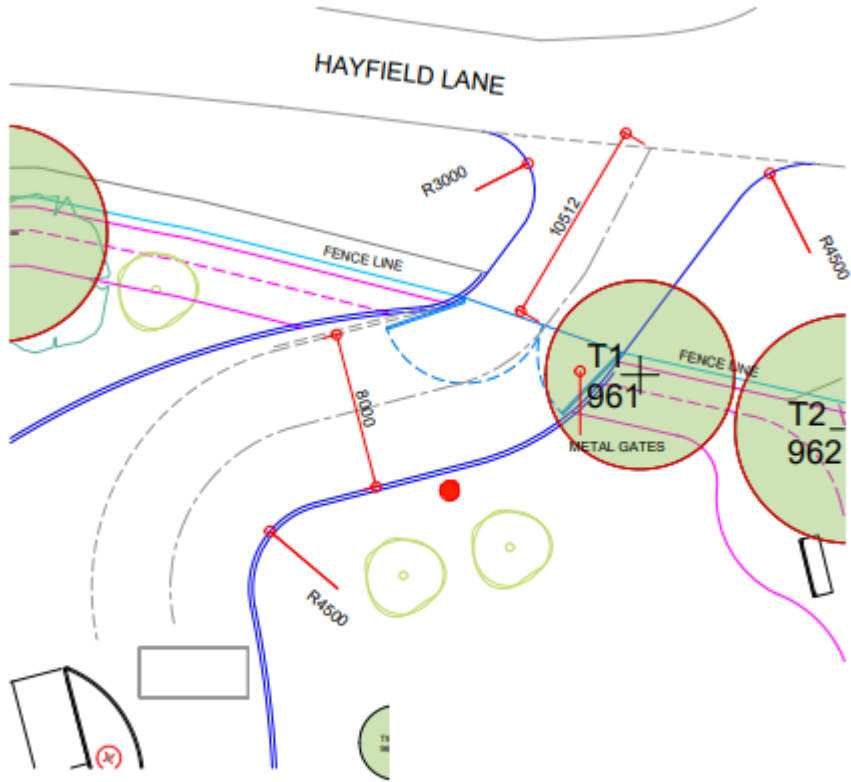


DATE	18/11/2015
DESIGNED BY	NEW HAYFIELD LANE TREATMENT FACILITY
DRAWN BY	PROPOSED SITE PLAN
CHECKED BY	ANTHONY SHOWN ARCHITECT 2011 RIBA
PROJECT NO.	1-7508A1
DATE	026
SHEET	15

ACCOMMODATION SCHEDULE

RECEPTION AND CHANGE	395 M SQ	1 NO PERMANEN
CAFE	60 M SQ	1 NO PERMANENT
CAMPING LODGE	336 M SQ	7 NO PORTABLE
2 bed allowed for in schedule		
TOILET AND SHOWER	48 M SQ	1 NO PORTABLE
TOTAL	839 M SQ	

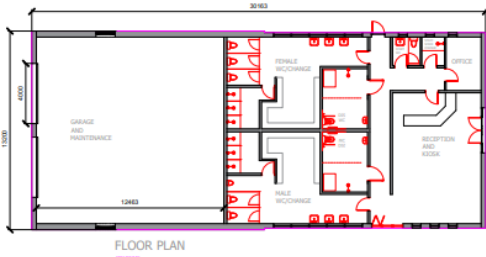
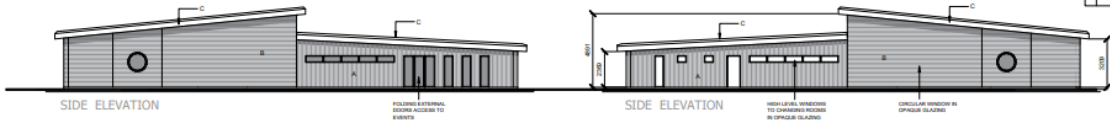
14	241122	TREE SURVEY LOCATIONS ADDED	AS	AS
15	010623	PROPOSED BUNDING REFORMED TO OUTSIDE OF ROOT PROTECTION AREA RPA	AS	AS



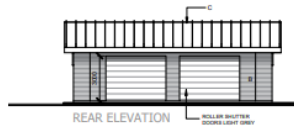
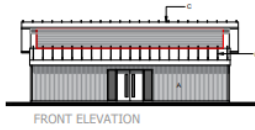
DP
001 SITE ENTRANCE
SCALE 1:200@A1

Appendix 3: Proposed Reception Building/Changing Facilities

NO	DATE	DESCRIPTION	BY	CHECKED
01	01/01/2023	ISSUED FOR PERMIT	AL	AL
02	01/01/2023	ISSUED FOR PERMIT	AL	AL



- MATERIALS**
- A VERTICAL TIMBER BOARDING / CLADDING IN MEDIUM GREY
 - B HORIZONTAL TIMBER BOARDING / CLADDING IN NATURAL TIMBER COLOUR
 - C STANDING SEAM PROFILED CLADDING LIGHT GREY
SHADE VERGES AND ROOFRITS IN PROFILED ALUMINIUM LIGHT GREY
 - WINDOWS AND DOORS UPVC ALUMINIUM LIGHT GREY
 - RAINWATER GOODS LIGHT GREY



RECEPTION AND CHANGE BUILDING

CLIENT AIR T LEWIS 100210102 NEW HAYFIELD LANE TRAFALGAR FACILITY		
PROJECT TITLE PROPOSED BUILDING RECEPTION / CHANGING		
PROJECT NUMBER 023		
SCALE 1:100		
PROJECT NO. 023	SHEET NO. 02	DATE 01/01/2023

Appendix 4: Proposed Café and Log Cabins

CAFE

Architectural drawings for a Café building, including elevations, floor plan, and roof plan. The drawings show a rectangular building with a pitched roof and a small entrance. The floor plan shows the interior layout with tables and chairs. The roof plan shows the roof structure. The elevations show the building from different angles, including a side elevation, front elevation, and rear elevation. A legend lists materials and finishes: A: VERTICAL TREKOR BOARDING / CLADDING IN MEDIUM GREY; B: STAINLESS STEEL PROFILED GLASSING LIGHT GREY; C: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; D: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; E: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; F: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; G: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; H: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; I: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; J: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; K: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; L: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; M: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; N: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; O: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; P: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; Q: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; R: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; S: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; T: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; U: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; V: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; W: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; X: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; Y: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; Z: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY.

NO.	DATE	DESCRIPTION	BY	CHKD
1	10/10/2023	ISSUED FOR PERMIT	AB	AB
2	10/10/2023	ISSUED FOR PERMIT	AB	AB

CAMPING LODGE

Architectural drawings for a Camping Lodge building, including elevations, floor plan, and roof plan. The drawings show a rectangular building with a pitched roof and a small entrance. The floor plan shows the interior layout with tables and chairs. The roof plan shows the roof structure. The elevations show the building from different angles, including a side elevation, front elevation, and rear elevation. A legend lists materials and finishes: A: VERTICAL TREKOR BOARDING / CLADDING IN MEDIUM GREY; B: STAINLESS STEEL PROFILED GLASSING LIGHT GREY; C: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; D: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; E: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; F: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; G: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; H: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; I: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; J: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; K: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; L: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; M: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; N: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; O: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; P: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; Q: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; R: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; S: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; T: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; U: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; V: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; W: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; X: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; Y: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY; Z: STAINLESS STEEL VERTICAL CLADDING LIGHT GREY.

NO.	DATE	DESCRIPTION	BY	CHKD
1	10/10/2023	ISSUED FOR PERMIT	AB	AB
2	10/10/2023	ISSUED FOR PERMIT	AB	AB

CLIENT AIR T. LEWIS PROJECT NEW HAYFIELD LAKE TRAVELON FACILITY	
DRAWING TITLE PROPOSED PLANS ELEVATIONS	
ARCHITECT SPINNEY BOWEN ARCHITECTS 88A LAURENCE ROAD LAURENCEVILLE VIC 3644	
SCALE 1:100	
PROJECT NO. 025	SHEET NO. 02

Appendix 5: Lighting Plan



KEY TO EXTERNAL LIGHTING TYPES

Architectural LED Area-Car Park Light (901)

Lighting Column
 CL01 X 2
 COLUMN HEIGHT 5M



AMENITY LIGHTING LOCATED ON BUILDING EXTERNAL ENVELOPES
 Part Number: 430152
 Rydal Wallpack 40W AND 70W 4000K Bronze
 AL01 FIXED TO BUILDING MAX HEIGHT 3M

Heavy Duty LED Floodlight (902)

Flood Light
 FL01
 FIXED TO
 COLUMN HEIGHT 5M

LED Bollard - Narrow beam (903)

Bollard Lighting
 BL01 X 2
 LOW LEVEL HEIGHT 1M

LED Flood - Narrow beam (904)


Caravan and Camping Hook Up Illuminated
 H01 LOW LEVEL HEIGHT 1M

Item	Code	Description	Quantity	Notes

DRAWN M T LEWIS		
PROJECT NEW WATFIELD LANE PROPOSED TRIATHLON FACILITY		
SHEET NO PROPOSED EXTERNAL LIGHTING		
DESIGNER O'NEILL DONOHUE ARCHITECT 3001 88A LONDON DATE 1-7500-A1		
PROJECT NO	DRAWING NO	SHEET NO
	030	01

KEY TO EXTERNAL LIGHTING TYPES

REV	DATE	DESCRIPTION	ISSUED	APPROVED
01	04/20	ISSUED FOR CONSULT AND APPROVAL	—	—



Architectural LED Area/Car Park Light (A05)

The Optimal LED is a high performance area light using LED optics illuminate large car parking areas. It is also designed as an LED replacement for WestLark's long established and popular Optima Car park luminaires, typically on a 1 for 1 basis.

For new Projects, typically 1 fitting will illuminate 112 parking spaces, in an area of 20M x 20M. Maximize column height (by maximizing spacing). Voltages in 400w LED. Symmetric and asymmetric /forward throw variant.

Construction: Car-Cast aluminium, flat glass optics (or no optics). Electric cable runs Centre Pipe into Tripod. IP66-Optical IP60-Drive Housing.

Installation: Tripod has integral Mast/trim - typical size: 0.9m x 1.0m x 1.0m high. The Luminaire is also available in [Fixed-leg Version without Tripod](#).

**LIGHTING COLUMN
CL01 X 2
COLUMN HEIGHT 5M**

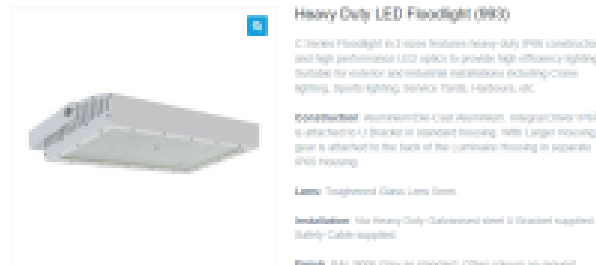


**AMENITY LIGHTING LOCATED ON BUILDING EXTERNAL ENVELOPE
Part Number: 430152
Rydal Wallpack 40W AND 70W 4000K Bronze AL01 FIXED TO BUILDING MAX HEIGHT 3M**

Diffuser Material: Polycarbonate
Mounting Type: Wall Mount
LED Information
LED Type: SMD
Lumen Output: ~10% 6200lm
Lumens Per Circuit Watt: 130Lum/W
LED Lifetime: L70: 50000 h
CCT: 4000K
CRI: >80
Electrical Information
Circuit Voltage: 40-50W
Connection Type: Screw Terminal
Class: Class I
Driver Information
Driver Brand: System
Dimming: Yes
Input Voltage: 230 - 240V AC
Operating Frequency: 50 - 60Hz
Power Factor: >0.90

Technical Specification

Certification
Manufactured in accordance to BS EN 60598-1
CE Marked: Yes
Product Dimensions
(B) 290 mm
(A) 220 mm
(C) 182 mm



Heavy Duty LED Floodlight (H02)

Compact Floodlight 6-12000 lumens heavy-duty IP66 construction and high performance LED optics to provide high efficiency lighting. Suitable for exterior and industrial installations including: Crane lighting, sports lighting, service yards, harbours, etc.

Construction: Aluminium die-cast aluminium, integrated driver unit is attached to a bracket on standard housing. Note: Larger housing gear is attached to the back of the luminaire housing in separate IP66 housing.

Lens: Toughened Glass Lens Glass

Installation: For Heavy Duty-Submersible and 2-Bracket supplied. Safety Cable supplied.

Finish:RAL 9005 Grey or standard. Other colours on request.

LED: Available in 140w - 240w LED. 40 or standard, 30 or 50 on request.

Moisture Protection: Various options:
- 2 Pin or 4 Pin IP66/IP67 versions
- 4 Pin or 6 Pin IP66/IP67 versions

Temperature: -20°C to +40°C

**FLOOD LIGHT
FL01
FIXED TO
COLUMN HEIGHT 5M**

**FLOOD LIGHT
FL02
FIXED TO BUILDING ST MAX 3 M HIGH**



LED Bollard, Service option (BL01)

LED bollard offers a variety of lighting applications including: pedestrian crossings, pedestrian crossings, pedestrian crossings.

Construction: Customised height 1000, 1200, 1400, 1600, 1800, 2000, 2200, 2400, 2600, 2800, 3000, 3200, 3400, 3600, 3800, 4000, 4200, 4400, 4600, 4800, 5000, 5200, 5400, 5600, 5800, 6000, 6200, 6400, 6600, 6800, 7000, 7200, 7400, 7600, 7800, 8000, 8200, 8400, 8600, 8800, 9000, 9200, 9400, 9600, 9800, 10000.

Installation: In-ground or surface.

Finish: Grey, RAL 9005, RAL 9006, RAL 9007, RAL 9008, RAL 9009, RAL 9010, RAL 9011, RAL 9012, RAL 9013, RAL 9014, RAL 9015, RAL 9016, RAL 9017, RAL 9018, RAL 9019, RAL 9020, RAL 9021, RAL 9022, RAL 9023, RAL 9024, RAL 9025, RAL 9026, RAL 9027, RAL 9028, RAL 9029, RAL 9030, RAL 9031, RAL 9032, RAL 9033, RAL 9034, RAL 9035, RAL 9036, RAL 9037, RAL 9038, RAL 9039, RAL 9040, RAL 9041, RAL 9042, RAL 9043, RAL 9044, RAL 9045, RAL 9046, RAL 9047, RAL 9048, RAL 9049, RAL 9050.

Weight: 10kg, 15kg, 20kg, 25kg, 30kg, 35kg, 40kg, 45kg, 50kg, 55kg, 60kg, 65kg, 70kg, 75kg, 80kg, 85kg, 90kg, 95kg, 100kg, 105kg, 110kg, 115kg, 120kg, 125kg, 130kg, 135kg, 140kg, 145kg, 150kg, 155kg, 160kg, 165kg, 170kg, 175kg, 180kg, 185kg, 190kg, 195kg, 200kg, 205kg, 210kg, 215kg, 220kg, 225kg, 230kg, 235kg, 240kg, 245kg, 250kg, 255kg, 260kg, 265kg, 270kg, 275kg, 280kg, 285kg, 290kg, 295kg, 300kg, 305kg, 310kg, 315kg, 320kg, 325kg, 330kg, 335kg, 340kg, 345kg, 350kg, 355kg, 360kg, 365kg, 370kg, 375kg, 380kg, 385kg, 390kg, 395kg, 400kg, 405kg, 410kg, 415kg, 420kg, 425kg, 430kg, 435kg, 440kg, 445kg, 450kg, 455kg, 460kg, 465kg, 470kg, 475kg, 480kg, 485kg, 490kg, 495kg, 500kg, 505kg, 510kg, 515kg, 520kg, 525kg, 530kg, 535kg, 540kg, 545kg, 550kg, 555kg, 560kg, 565kg, 570kg, 575kg, 580kg, 585kg, 590kg, 595kg, 600kg, 605kg, 610kg, 615kg, 620kg, 625kg, 630kg, 635kg, 640kg, 645kg, 650kg, 655kg, 660kg, 665kg, 670kg, 675kg, 680kg, 685kg, 690kg, 695kg, 700kg, 705kg, 710kg, 715kg, 720kg, 725kg, 730kg, 735kg, 740kg, 745kg, 750kg, 755kg, 760kg, 765kg, 770kg, 775kg, 780kg, 785kg, 790kg, 795kg, 800kg, 805kg, 810kg, 815kg, 820kg, 825kg, 830kg, 835kg, 840kg, 845kg, 850kg, 855kg, 860kg, 865kg, 870kg, 875kg, 880kg, 885kg, 890kg, 895kg, 900kg, 905kg, 910kg, 915kg, 920kg, 925kg, 930kg, 935kg, 940kg, 945kg, 950kg, 955kg, 960kg, 965kg, 970kg, 975kg, 980kg, 985kg, 990kg, 995kg, 1000kg.

Operating Temperature: 0°C to 40°C

LED: 1 x 10000lm LED Module with 4000K or standard. Customised lighting 2000.

LED Driver: Mean-Drive, Mean-Drive, Mean-Drive.

Options: Customised colour, emergency, standard, 1-2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100.

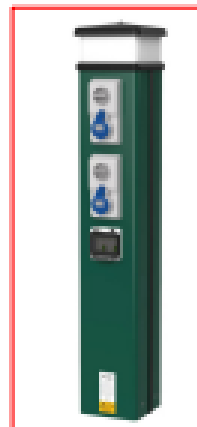
Product Compliance: CE, RoHS, LVD.

Product Label: Download Product Label

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**BOLLARD LIGHTING
BL01 X 2
LOW LEVEL HEIGHT 1M**



**CARAVAN AND CAMPING HOOK UP ILLUMINATED
H01 LOW LEVEL HEIGHT 1M**

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PROJECT TITLE NEW HAYFIELD LANE PROPOSED TRIATHLON FACILITY
ISSUING TITLE PROPOSED EXTERNAL LIGHTING
ARCHITECT ANTHONY DUNN ARCHITECT 2020 1 PEARSON COURT SHEFFIELD
SCALE 1:1000

ANTHONY DUNN ARCHITECT